

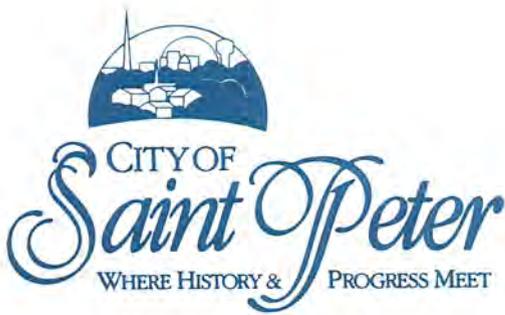
**CITY OF SAINT PETER, MINNESOTA  
AGENDA AND NOTICE OF MEETING**

Regular Workshop Session of Monday, April 7, 2014  
Library Meeting Room– 6:00 p.m.  
601 South Washington Avenue

- I. **CALL TO ORDER**
- II. **DISCUSSION**
  - A. Frozen Water Service Line Update
  - B. Construction Standard Details Update
  - C. League of Minnesota Cities Annual Conference
  - D. Goal Session Schedule
  - E. Others
- III. **ADJOURNMENT**

TP/bal

Office of the City Administrator  
Todd Prafke



## Memorandum

**TO:** Honorable Mayor Strand  
Members of the City Council

**DATE:** 4/4/2014

**FROM:** Todd Prafke  
City Administrator

**RE:** Frozen Water Service Lines

### ACTION/RECOMMENDATION

None needed. For your information and discussion only.

### BACKGROUND

The current policy of the City, which is contained in the City Code, is that frozen service lines are the responsibility of the property owner, not the City. That means that the property owner is responsible for the line to be thawed. If the City thaws the line, the charge is \$325, but only if it is thawed. There isn't a charge to the customer if our effort does not work and the customer has to hire a certified welder to open the line. If we cannot thaw the line, customers are provided with a list of qualified welders who are doing this type of work. City staff also coordinates this private work if they want the help, but we don't hire or pay the welder. It generally costs about \$200 for set up and then about \$65 per hour for the heating. An hour or two gets it done. We also provide staff support for this and do not charge for that support.

Here is what we are telling or have told people who have been concerned about their bills:

*"The current policy of the City is that, unfortunately we cannot pay for your water or sewer use as a result of the water continuing to run to minimize your potential of a freeze up. The Council has directed us to gather data about this which may include things like how many homes are impacted (right now we are over 50), the amount of water used, and specifics about each individual property because we know not all properties are the same. If you are unable to pay your entire bill we will be happy to offer a payment plan as we know this can increase your cost significantly. The Council will receive this information in a couple of weeks and decide the next steps, if any. You do not need to contact us again as we have a record of your situation and we will contact you should the Council determine it wishes to change the policy."*

What I see is about a 50/50 split of cities offering some type of relief for frozen water services. They type of relief offered is all over the board. Some offer a specified number of gallons of discount, some use an average and pay for anything over the average. Some attack this by offering to pay all or a portion of the thawing cost, however this increases the risk for future claims so we have steered away from this liability. Some will provide incentive to dig the lines

deeper to avoid the problem in the future and again about 50% have a similar policy to what the City of Saint Peter currently has.

Your crews have worked very hard over very long hours to help our customers, but you have some customers that are angry about the situation they are in. I think your challenge will be to determine if you want to help the 72 property owners who have experienced problems at the expense of the rest of the rate payers. Any solution may also depend on identifying the problem you are trying to fix. If we are trying to make folks happy, give them a discount or money or pay for the thawing (each of these impacts customers differently depending on a number of factors including whether they own or rent). If your goal is a long-term solution to avoid these problems, then maybe an incentive to put service lines deeper is the best way to go. (Again, that impacts different customers very differently.)

Staff has assembled some background information related to the 72 services that did experience trouble this winter. Staff will be available at the workshop to discuss a matrix of data that will be distributed at the workshop. The information was not available for your packet as we were still gathering information at the packet deadline.

The information will include the following data and maybe more:

- Number of frozen services which is currently at 72.
- Amount of additional water used by frozen services.
- Some additional sense of categories frozen lines might fall into.
- Sense of services used to thaw and the number of each.
- Some type of recommendation as to how to approach this situation.

Please let me know if you have additional questions or concerns on this agenda item.

TP/bal



## Memorandum

**TO:** Todd Prafke  
City Administrator

**DATE:** 04/02/14

**FROM:** Lewis Giesking *Lew*  
Director of Public Works

**RE:** Standard Detail Updates for 2014

### ACTION/RECOMMENDATION

None needed for your discussion only.

### BACKGROUND

The City has standard details that are used for construction of public infrastructure through out the City. Recently the American Disabilities Act (ADA) required changes in public standards; it is the City's intention to stay in compliance with ADA requirements and the changes are reflected in the following City standards.

#### **Standard Detail Number**

#### **Modification**

0001 –Eccentric Fernco Splint

Added: #5 Rebar on each side of pipe (TYP.) for a total of four (4) each.

0002 – Concentric Fernco Splint  
Dome area adjacent to curb

Added: #5 Rebar

1001 – Bituminous Trail Detail

Added: or recycled concrete or bituminous

1003 – Concrete Walk Typical Section

Added: or recycled concrete or bituminous

1005 – Concrete Trail

Added: or recycled concrete or bituminous

2001 – Stainless Steel Pole Street Light

Added: LED Head – Phillips Lumec catalog  
NO. 160W96LEDK-R-LE2-UNIV-RC-BK  
Factory finish – Black  
With 20 Year life photo eye

4006 – Storm Sewer Design F Manhole

Added: Note 3 – Tongue and groove joint

7017 – Pedestrian Curb Ramp with Truncated Dome area

Reworded Note #11 – The construction of a new or reconstruction of an existing pedestrian ramp will not be allowed unless a pedestrian ramp meeting city standards is located on the opposite side of the street from the ramp being proposed. In the event no ramp exists a new ramp meeting city standards shall be constructed (See detail 7017B)

Added: Note #13

Added: Note #14

Added: Note #15

Added: Note #16

Added: Legend – These longitudinal slope ranges shall be the starting point. If site conditions warrant, longitudinal slope s up to 8.3% for flatter are allowed

Added: Note S

Added: Note F

7017A – Pedestrian Curb Ramp with Sidewalk Adjacent to Curb

Added: or recycled concrete or bituminous

Added: Legend – These longitudinal slope ranges shall be the starting point. If site conditions warrant, longitudinal slope s up to 8.3% for flatter are allowed

Added: Note S

Added: Note F

Added: Note #1

Added: Note #2

Added: Note #3

Added: Note #5

7017B – Intersection Pedestrian Ramp and Crosswalk Requirements

New

7020A – Residential/Commercial Concrete Walk and apron adjacent to curb

Added: or recycled concrete or bituminous

7020 C – Commercial Concrete Walk and Apron

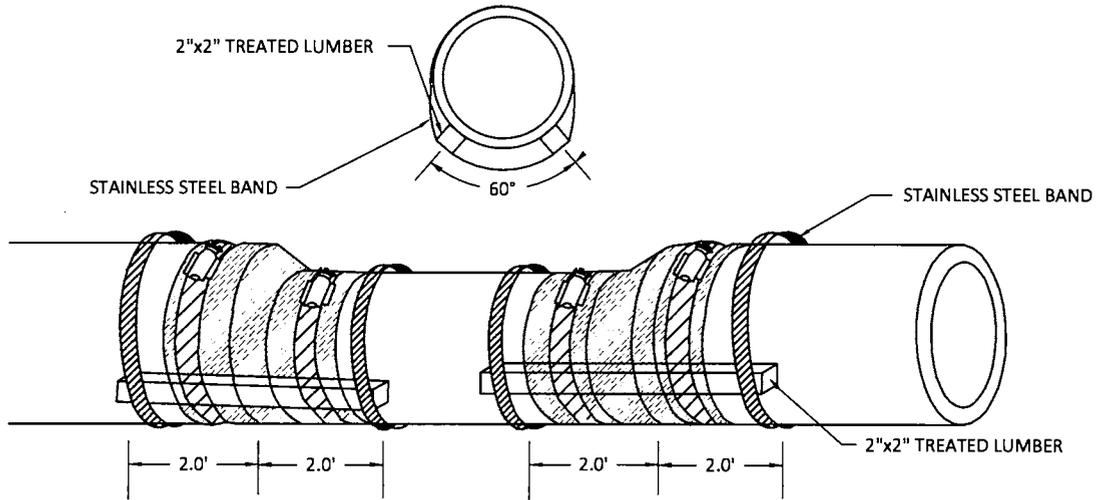
Added: or recycled concrete or bituminous

7020R – Residential Concrete Walk and Apron

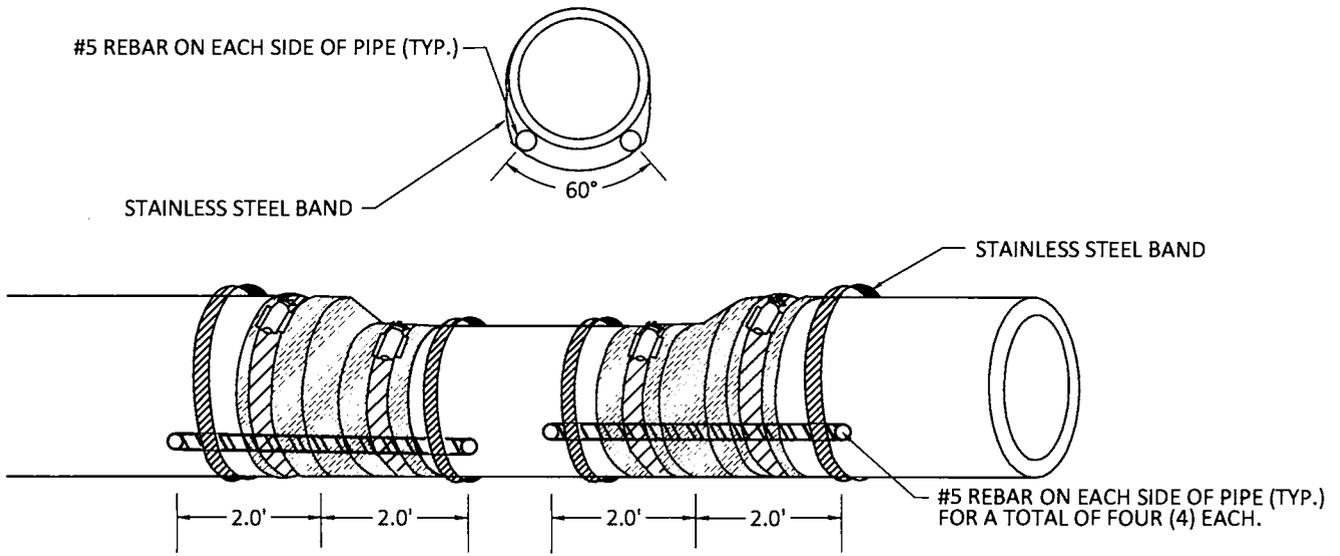
Added: or recycled concrete or bituminous

Water Utilities Superintendent Moulton assisted with the preparation of this memo. Please feel free to contact me should you have any questions or concerns about this agenda item.

AK/PTM/LGG/vwt



2"x2" TREATED LUMBER



#5 REBAR

ECCENTRIC FERNCO SPLINT  
NOT TO SCALE  
SAINT PETER STANDARD  
PLATE 0001

**ECCENTRIC FERNCO SPLINT**

STANDARD DETAIL  
PLATE NO:  
**0001**

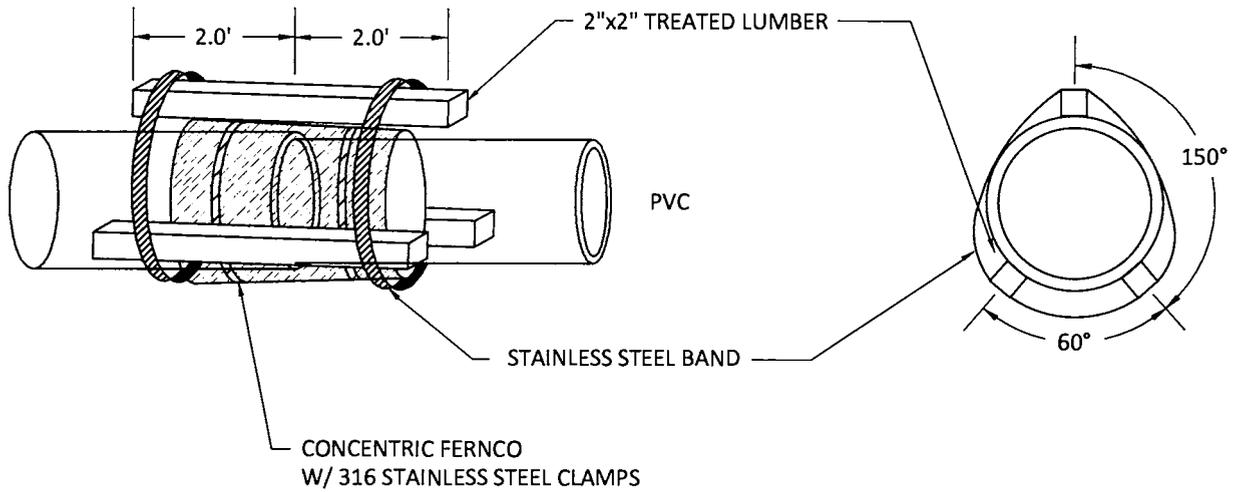
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REVISED:

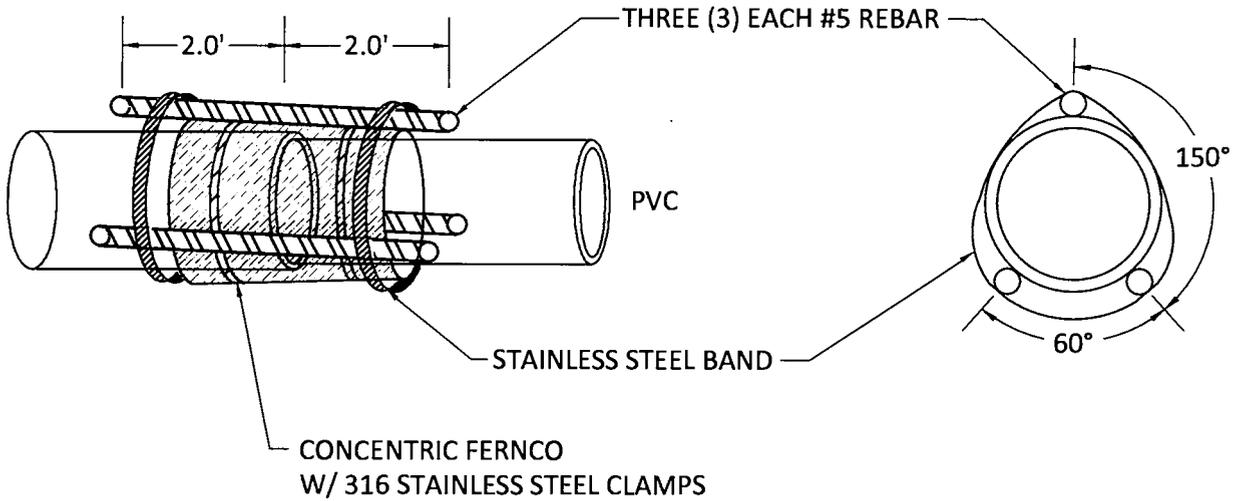
REMARKS:

**SAINT PETER UTILITY STANDARD DETAIL**





2"x2" TREATED LUMBER



#5 REBAR

CONCENTRIC FERNCO SPLINT

NOT TO SCALE

SAINT PETER STANDARD  
PLATE 0002

**CONCENTRIC FERNCO SPLINT**

STANDARD DETAIL  
PLATE NO:  
**0002**

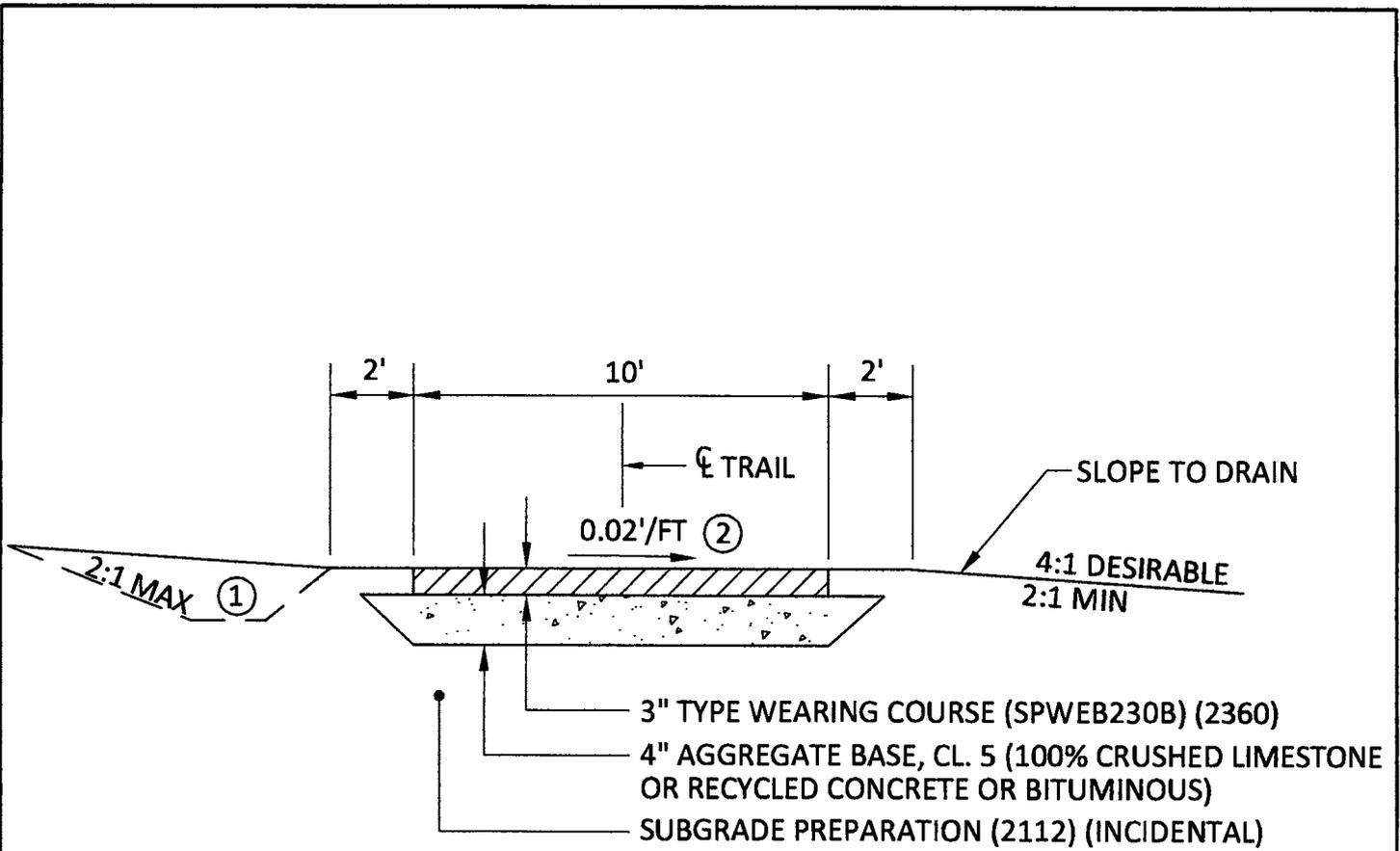
APPROVED: ADOPTED BY CITY COUNCIL ON JANUARY 12, 2004

REVISED:

REMARKS:

**SAINT PETER UTILITY STANDARD DETAIL**



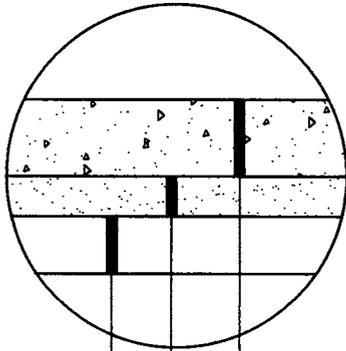


- ① DITCH IF REQUIRED FOR DRAINAGE
- ② SLOPE TOWARDS INSIDE OF CURVE

**BITUMINOUS TRAIL DETAIL**  
 NOT TO SCALE  
 SAINT PETER STANDARD  
 PLATE 1001

<b>BITUMINOUS TRAIL DETAIL</b>	STANDARD DETAIL PLATE NO: <b>1001</b>
APPROVED: ADOPTED BY CITY COUNCIL ON JANUARY 12, 2004	 <b>CITY OF Saint Peter</b> <small>NICOLLET COUNTY, MINNESOTA</small> <small>WHERE HISTORY &amp; PROGRESS MEET</small>
REVISED: REVISION ADOPTED BY CITY COUNCIL ON AUGUST 8, 2011	
REMARKS:	
<b>SAINT PETER UTILITY STANDARD DETAIL</b>	

Y:\STPE\1001stpe - Revised Detail.dwg



4" CONC. SIDEWALK (2521)  
 3" AGGREGATE BASE, CL. 5 (2211)  
 (100% CRUSHED LIMESTONE OR RECYCLED CONCRETE OR BITUMINOUS)  
 (INCIDENTAL TO WALK)  
 SUBGRADE PREPARATION (2112) (INCIDENTAL)

**NOTES:**

1. MINNESOTA DEPARTMENT OF TRANSPORTATION STANDARD SPECIFICATIONS FOR CONSTRUCTION SHALL APPLY. CONCRETE MIX 3A32 WILL BE REQUIRED.
2. SIDEWALKS: REFERENCE SECTION 2521, AGGREGATE BASE: REFERENCE SECTION 2211
3. 1/2" PRE FORMED JOINT FILLER MATERIAL-AASHTO M213 REQUIRED AT THE INTERSECTION OF ALL MAINLINE WALK WITH STATIONARY OBJECTS. (I.E., BUILDING FOUNDATION, EXISTING WALK, ETC.) (NOT REQUIRED WHEN CURB AND GUTTER OR WALK ARE ADJACENT TO BITUMINOUS PAVEMENT.)
4. PLACE 1/2" EXPANSION JOINT AT 100 FT. INTERVALS FOR SIDEWALK.
5. CONCRETE WALK CONSTRUCTION JOINTS SHALL BE TOOLED OR SAW CUT AT 6 FT. INTERVALS.
6. SIDEWALK EDGES SHALL BE ROUNDED WITH 1/4" RADIUS TOOL.

**CONCRETE WALK**  
 NOT TO SCALE  
**SAINT PETER STANDARD**  
**PLATE 1003**

**CONCRETE WALK**  
**TYPICAL SECTION**

STANDARD DETAIL  
 PLATE NO:  
**1003**

APPROVED: ADOPTED BY CITY COUNCIL ON JANUARY 12, 2004

REVISED: REVISION ADOPTED BY CITY COUNCIL ON MAY 14, 2012

REMARKS:

**SAINT PETER UTILITY STANDARD DETAIL**



2' CLEAR ZONE

2' CLEAR ZONE

10'

CL TRAIL

0.02'/FT

SLOPE TO DRAIN

4:1 MAX

4" CONC. PAVEMENT (2531)

4" AGGREGATE BASE, CL. 5 (2211)

(100% CRUSHED LIMESTONE OR RECYCLED CONCRETE OR BITUMINOUS)  
(INCIDENTAL TO TRAIL)

SUBGRADE PREPARATION (2112) (INCIDENTAL)

**NOTES:**

1. ALL JOINTS SHALL BE SAW CUT TO A DEPTH OF  $1\frac{3}{16}$ ".
2. TRAIL CENTERLINE JOINT SHALL BE SAW CUT.
3. TRANSVERSE JOINTS SHALL BE SAW CUT AT 5 FT. INTERVALS.
4. TRAIL EDGES SHALL BE ROUNDED WITH  $\frac{1}{4}$ " RADIUS TOOL.
5.  $\frac{1}{2}$ " EXPANSION JOINT SHALL BE CONSTRUCTED AT MAXIMUM 100 FT. INTERVALS.

**CONCRETE TRAIL**

NOT TO SCALE

SAINT PETER STANDARD

PLATE 1005

**CONCRETE TRAIL**

STANDARD DETAIL

PLATE NO:

**1005**

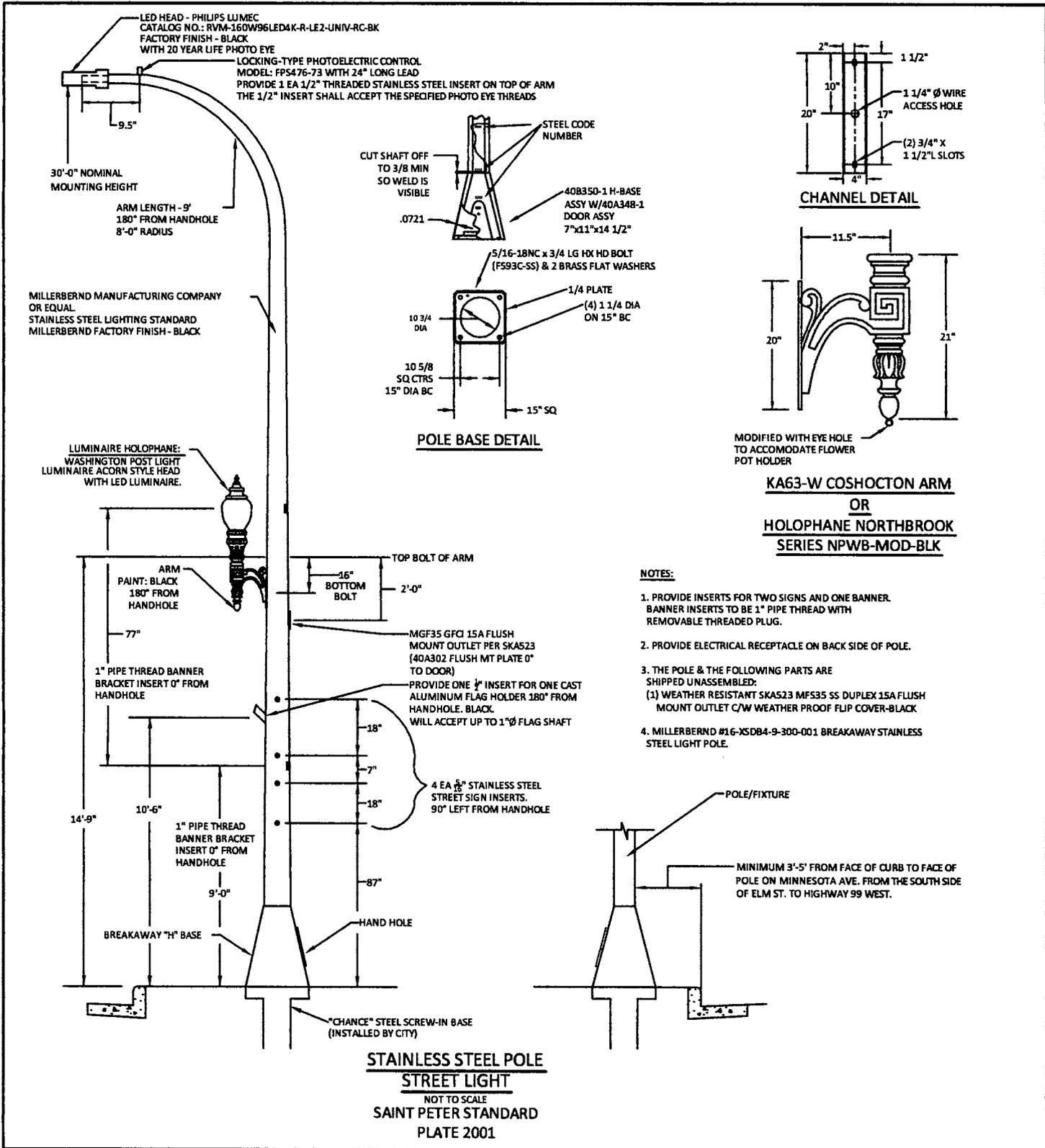
APPROVED: ADOPTED BY CITY COUNCIL ON MAY 14, 2012

REVISED:

REMARKS:

**SAINT PETER UTILITY STANDARD DETAIL**





# STAINLESS STEEL POLE STREET LIGHT

STANDARD DETAIL  
 PLATE NO:  
**2001**

APPROVED: ADOPTED BY CITY COUNCIL ON MARCH 23, 2009

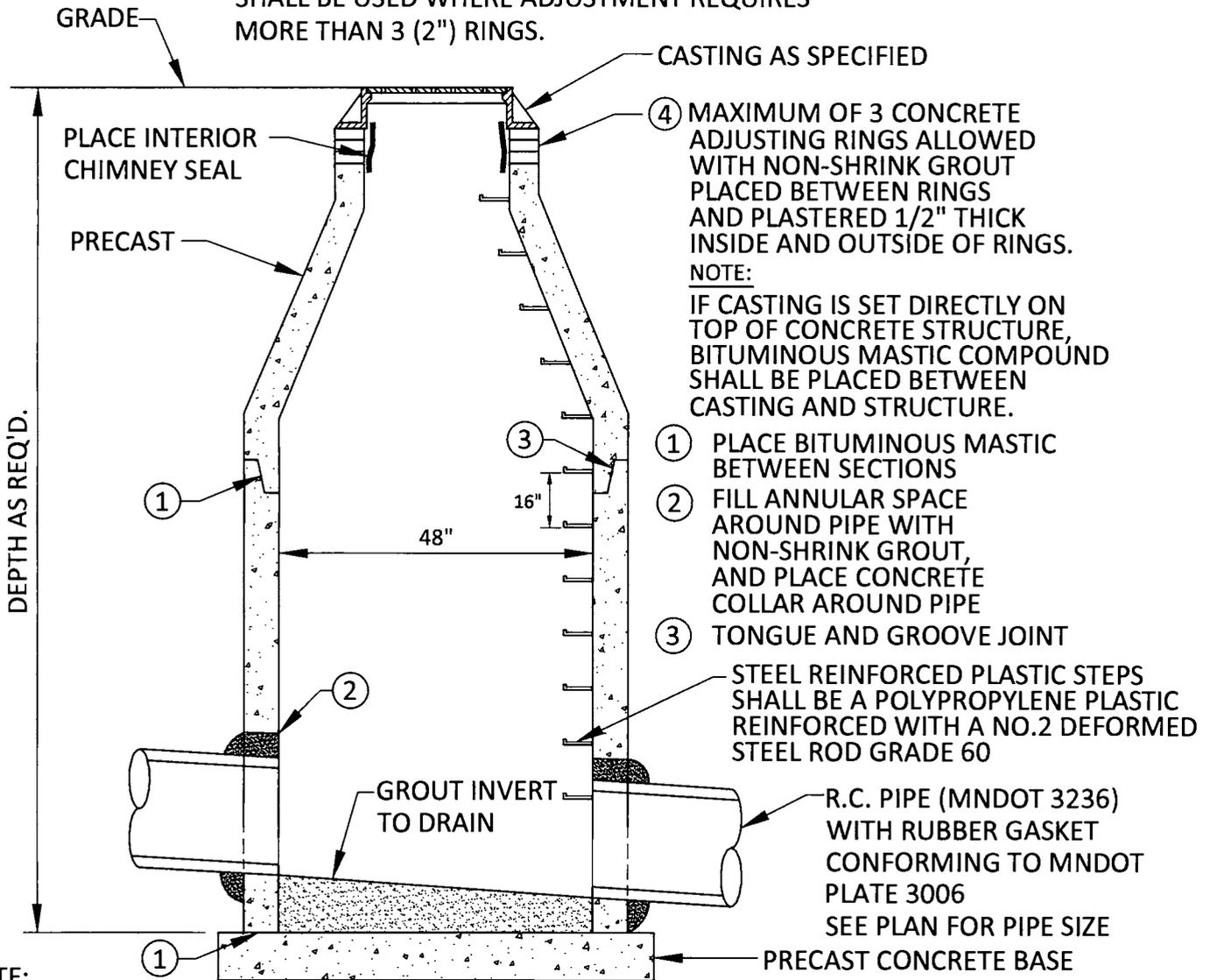
REVISED: REVISION ADOPTED BY CITY COUNCIL ON

REMARKS: COMMERCIAL

## SAINT PETER UTILITY STANDARD DETAIL



④ MAXIMUM ADJUSTMENT ALLOWED BETWEEN THE TOP CONE SECTION AND BOTTOM CASTING IS 12". A MAXIMUM OF 3 INDIVIDUAL ADJUSTING RINGS SHALL BE USED. TALLER 6" OR 12" RINGS SHALL BE USED WHERE ADJUSTMENT REQUIRES MORE THAN 3 (2") RINGS.



NOTE:

THE PIPE END(S) SHALL BE STUBBED INSIDE THE MANHOLE OR CATCH BASIN A MAXIMUM OF THREE INCHES (3"), UNLESS APPROVED OTHERWISE BY THE ENGINEER/OWNER.

### STORM SEWER DESIGN F MANHOLE

NOT TO SCALE  
SAINT PETER STANDARD  
PLATE 4006

## STORM SEWER DESIGN F MANHOLE

STANDARD DETAIL  
PLATE NO:  
**4006**

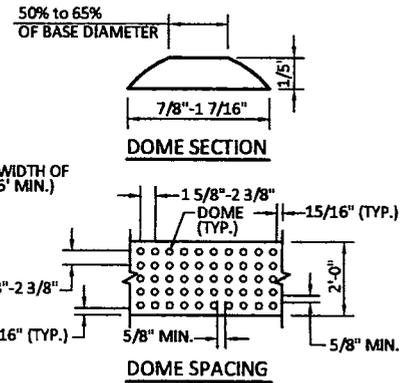
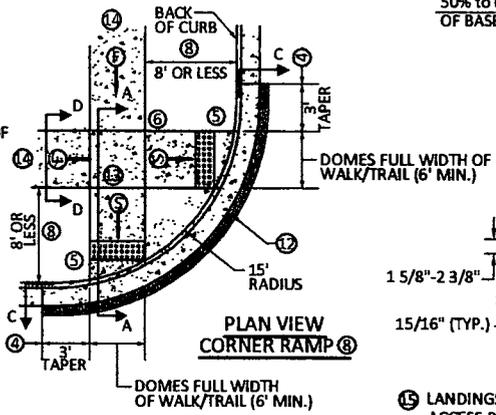
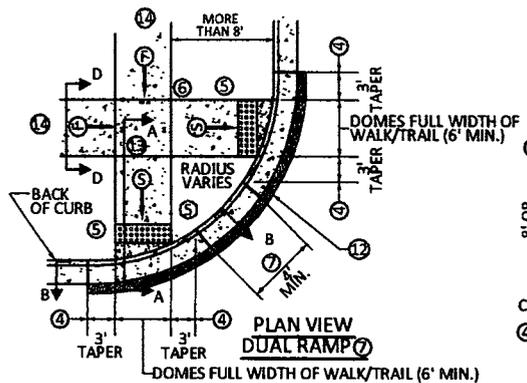
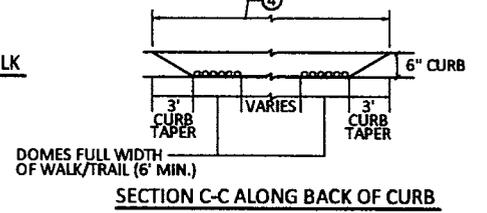
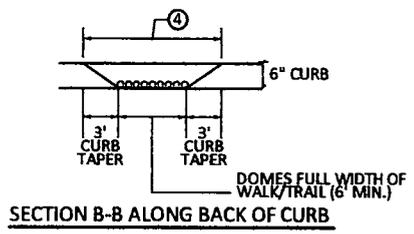
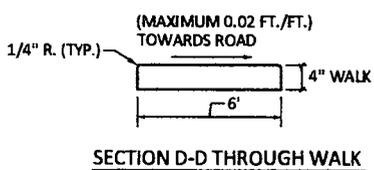
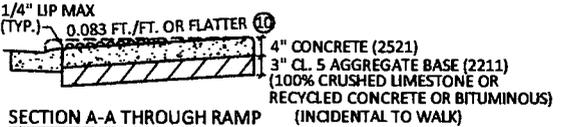
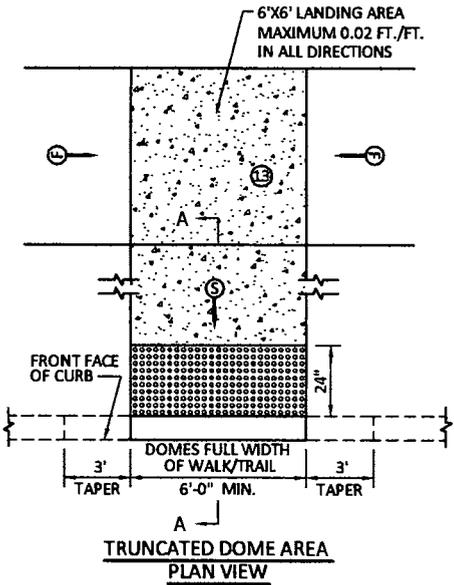
APPROVED: ADOPTED BY CITY COUNCIL ON JANUARY 12, 2004

REVISED: REVISION ADOPTED BY CITY COUNCIL ON FEBRUARY 26, 2007

REMARKS:

**SAINT PETER UTILITY STANDARD DETAIL**





**NOTES:**

- ① THE CURB AND CURB TRANSITION ON THE RAMP WILL BE PAID FOR AS UNEAR FEET OF CONCRETE CURB OR CONCRETE CURB AND GUTTER. THE RAMP AREA WILL BE PAID FOR AS CONCRETE WALK. THE TRUNCATED DOME AREA SHALL BE PAID BY THE SQ. FT.
- ② MAXIMUM OFFSET OF THE DETECTABLE WARNING PLATES/TRUNCATED DOME AREA FROM THE BACK OF CURB IS 6\"/>

⑫ SAW CUT AND REMOVE EXISTING BITUMINOUS PAVEMENT 6\"/>

- ⑬ LANDINGS SHALL BE LOCATED ANYWHERE THE PEDESTRIAN ACCESS ROUTE CHANGES DIRECTION. AT THE TOP OF RAMPS THAT HAVE RUNNING SLOPES GREATER THAN 5.0%, AND IF THE APPROACHING WALK IS INVERSE GRADE INITIAL CURB RAMP LANDINGS SHALL BE CONSTRUCTED WITHIN 15' FROM THE BACK OF CURB, WITH 6' FROM THE BACK OF CURB BEING THE PREFERRED DISTANCE. SECONDARY CURB RAMP LANDINGS ARE REQUIRED FOR EVERY 30\"/>

**LEGEND**

- ⑭ THESE LONGITUDINAL SLOPE RANGES SHALL BE THE STARTING POINT. IF SITE CONDITIONS WARRANT, LONGITUDINAL SLOPES UP TO 8.3% OR FLATTER ARE ALLOWED.
- ⑮ INDICATES PEDESTRIAN RAMP - SLOPE SHALL BE BETWEEN 5.0% MINIMUM AND 8.3% MAXIMUM IN THE DIRECTION SHOWN AND THE CROSS SLOPE SHALL NOT EXCEED 2.0%
- ⑯ INDICATES PEDESTRIAN RAMP - SLOPE SHALL BE GREATER THAN 2.0% AND LESS THAN 5.0% IN THE DIRECTION SHOWN AND CROSS SLOPE SHALL NOT EXCEED 2.0%

**PEDESTRIAN CURB RAMP**  
NOT TO SCALE  
SAINT PETER STANDARD  
PLATE 7017

**PEDESTRIAN CURB RAMP WITH TRUNCATED DOME AREA**

STANDARD DETAIL  
PLATE NO:  
**7017**

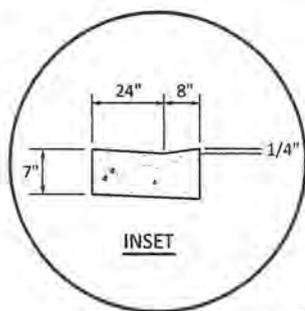
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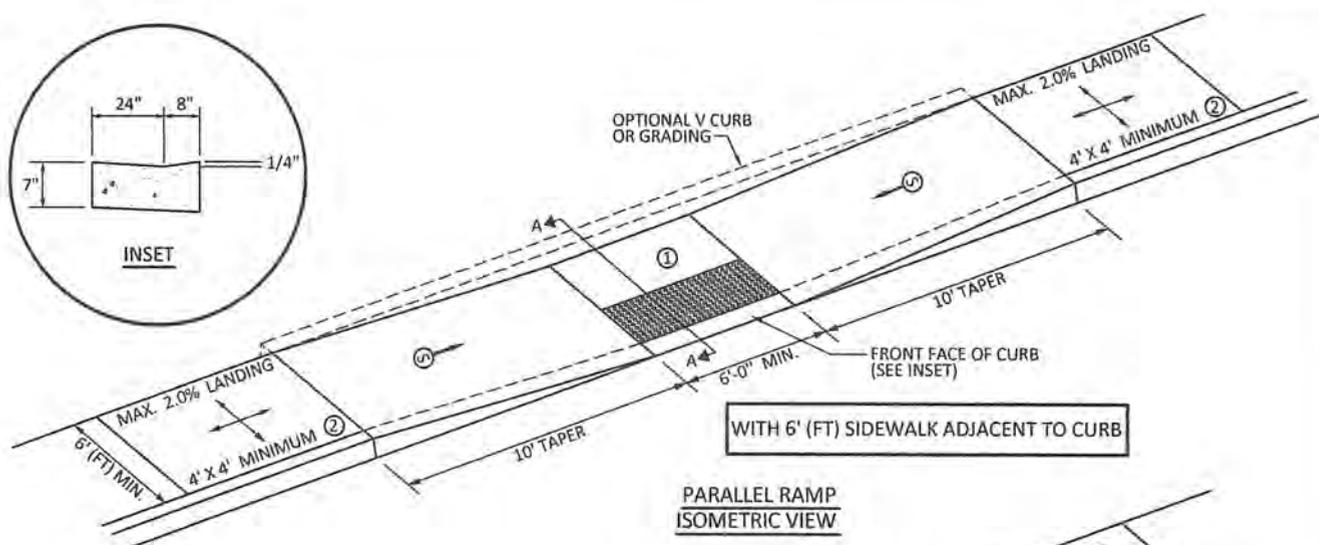
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**SAINT PETER UTILITY STANDARD DETAIL**



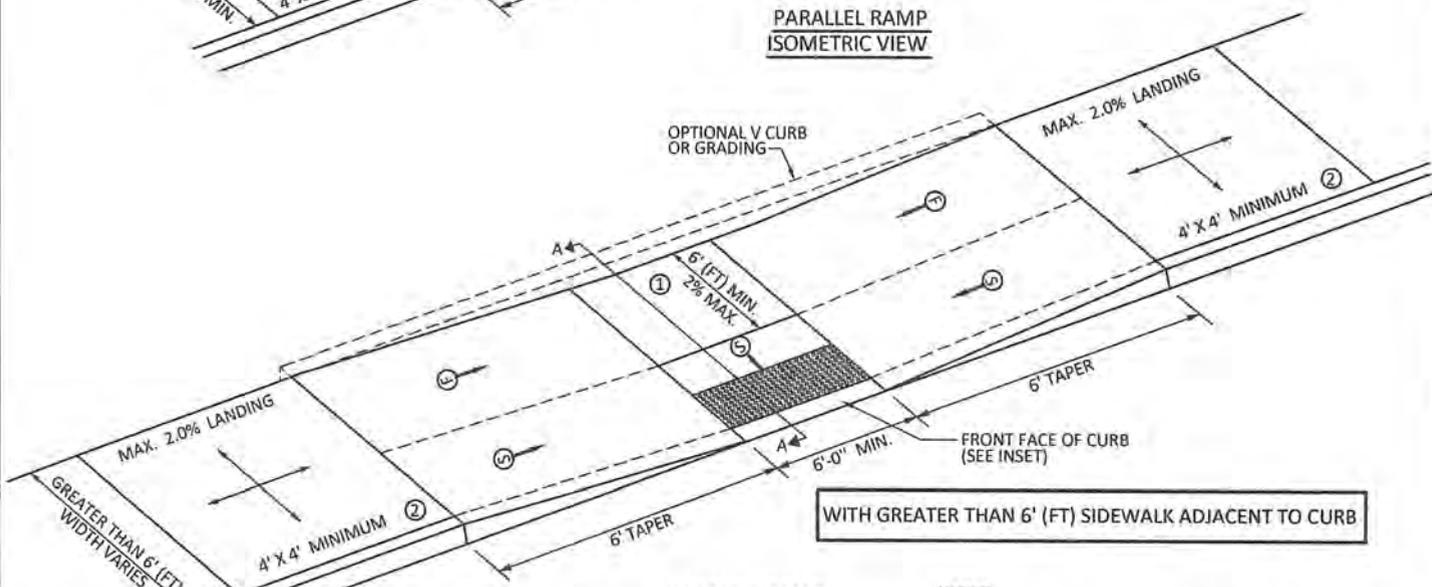


**INSET**



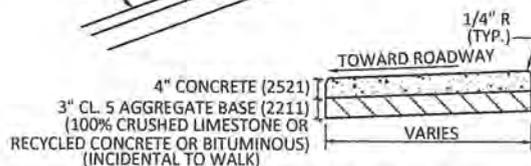
**WITH 6' (FT) SIDEWALK ADJACENT TO CURB**

**PARALLEL RAMP  
ISOMETRIC VIEW**



**WITH GREATER THAN 6' (FT) SIDEWALK ADJACENT TO CURB**

**PARALLEL RAMP  
ISOMETRIC VIEW**



**SECTION A-A THROUGH WALK**

**LEGEND**

THESE LONGITUDINAL SLOPE RANGES SHALL BE THE STARTING POINT. IF SITE CONDITIONS WARRANT, LONGITUDINAL SLOPES UP TO 8.3% OR FLATTER ARE ALLOWED.

⑤ INDICATES PEDESTRIAN RAMP - SLOPE SHALL BE BETWEEN 5.0% MINIMUM AND 8.3% MAXIMUM IN THE DIRECTION SHOWN AND THE CROSS SLOPE SHALL NOT EXCEED 2.0%

⑥ INDICATES PEDESTRIAN RAMP - SLOPE SHALL BE GREATER THAN 2.0% AND LESS THAN 5.0% IN THE DIRECTION SHOWN AND CROSS SLOPE SHALL NOT EXCEED 2.0%

**PEDESTRIAN CURB RAMP  
WITH SIDEWALK  
ADJACENT TO CURB**  
NOT TO SCALE  
SAINT PETER STANDARD  
PLATE 7017A

**NOTES:**

1. 4' BY 4' MIN. LANDING WITH MAX. 2% SLOPE IN ALL DIRECTIONS.
2. IF LONGITUDINAL SLOPE IS GREATER THAN 5.0%, 4' X 4' MIN. LANDING WITH MAX 2.0% SLOPE IN ALL DIRECTIONS REQUIRED.
3. FOR CONSTRUCTION NOTES, REFER TO DETAILS 7017.
4. LANDINGS SHALL BE LOCATED ANYWHERE THE PEDESTRIAN ACCESS ROUTE CHANGES DIRECTION, AT THE TOP OF RAMPS THAT HAVE RUNNING SLOPES GREATER THAN 5.0%, AND IF THE APPROACHING WALK IS INVERSE GRADE SECONDARY CURB RAMP LANDINGS ARE REQUIRED FOR EVERY 30" OF VERTICAL RISE WHEN THE LONGITUDINAL SLOPE IS GREATER THAN 5.0%.  
CONTRACTION JOINTS SHALL BE CONSTRUCTED ALONG ALL GRADE BREAKS. ALL GRADE BREAKS WITHIN THE PEDESTRIAN ACCESS ROUTE (PAR) SHALL BE PERPENDICULAR TO THE PATH OF TRAVEL.  
DETECTABLE WARNINGS MAY BE PART OF 6' X 6' LANDING AREA IF IT IS NOT FEASIBLE TO CONSTRUCT THE LANDING OUTSIDE OF THE DETECTABLE WARNING AREA.
5. SEE MNDOT STANDARD PLAN SHEET NO. 5-297.250 (SHEETS 1 THRU 5) FOR ADDITIONAL DETAIL INFORMATION.

**PEDESTRIAN CURB RAMP  
WITH SIDEWALK  
ADJACENT TO CURB**

STANDARD DETAIL  
PLATE NO:  
**7017A**

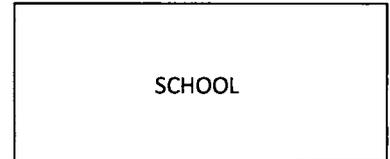
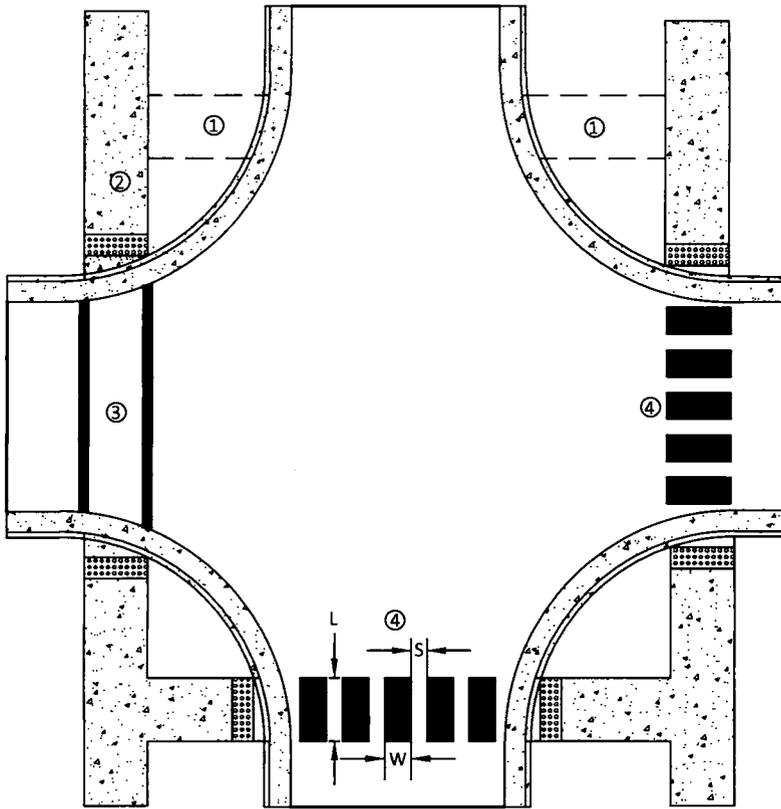
APPROVED: ADOPTED BY CITY COUNCIL ON APRIL 22, 2013

REVISED: ADOPTED BY CITY COUNCIL ON \_\_\_\_\_

REMARKS:

**SAINT PETER UTILITY STANDARD DETAIL**





**ZEBRA CROSSWALK MARKINGS**

**NOTES:**

- ① PEDESTRIAN RAMP NOT REQUIRED UNLESS RAMP ON OPPOSITE SIDE OF ROAD IS CONSTRUCTED OR RECONSTRUCTED.
- ② NEWLY CONSTRUCTED OR RECONSTRUCTED PEDESTRIAN RAMPS SHALL MEET REQUIREMENTS OF STANDARD DETAILS 7017 AND 7017A.
- ③ 6" WHITE STRIPING REQUIRED AT CROSS WALK OF COLLECTOR ROAD. STRIPING NOT REQUIRED ON LOCAL (NON-COLLECTOR) STREETS.
- ④ ZEBRA CROSSWALK STRIPING REQUIRED AT ALL CROSS WALKS ADJACENT TO SCHOOLS REGARDLESS OF COLLECTOR OR LOCAL CLASSIFICATION. SEE ZEBRA CROSSWALK MARKINGS TABLE AND NOTES.

WIDTH OF ROAD (FACE TO FACE)	(W) WIDTH OF PAINTED AREA	(S) WIDTH OF SPACE	(L) LENGTH OF PAINTED AREA	NUMBER OF PAINTED AREAS
36 FEET	2.5 FEET	3.5 FEET	6.0 FEET	5 EACH
40 FEET	2.5 FEET	2.5 FEET	6.0 FEET	7 EACH
44 FEET	2.5 FEET	3.0 FEET	6.0 FEET	7 EACH

- Ⓐ PAINTED AREAS TO BE CENTERED ON CENTERLINE AND LANE LINES.
- Ⓑ A MINIMUM OF 1.5 FEET CLEAR DISTANCE SHALL BE LEFT ADJACENT TO THE CURB. IF THE LAST PAINTED AREA FALLS INTO THIS DISTANCE, IT MUST BE OMITTED.
- Ⓒ THE BLOCKS SHALL BE LOCATED TO AVOID THE WHEEL PATH OF VEHICLES.

**PEDESTRIAN RAMP & CROSSWALK**

NOT TO SCALE  
 SAINT PETER STANDARD  
 PLATE 7017B

**INTERSECTION PEDESTRIAN RAMP AND CROSSWALK REQUIREMENTS**

STANDARD DETAIL  
 PLATE NO:  
**7017B**

APPROVED: ADOPTED BY CITY COUNCIL ON \_\_\_\_\_

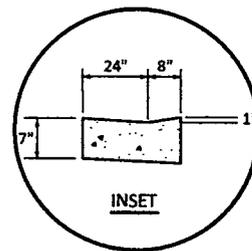
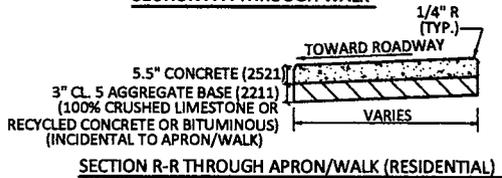
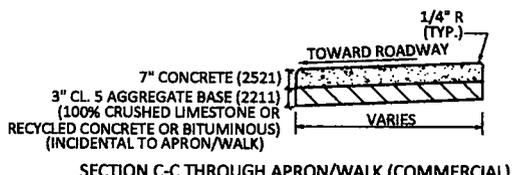
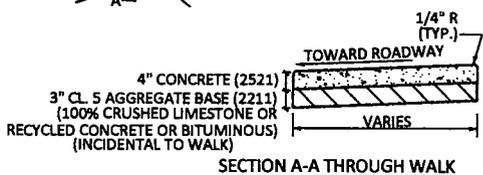
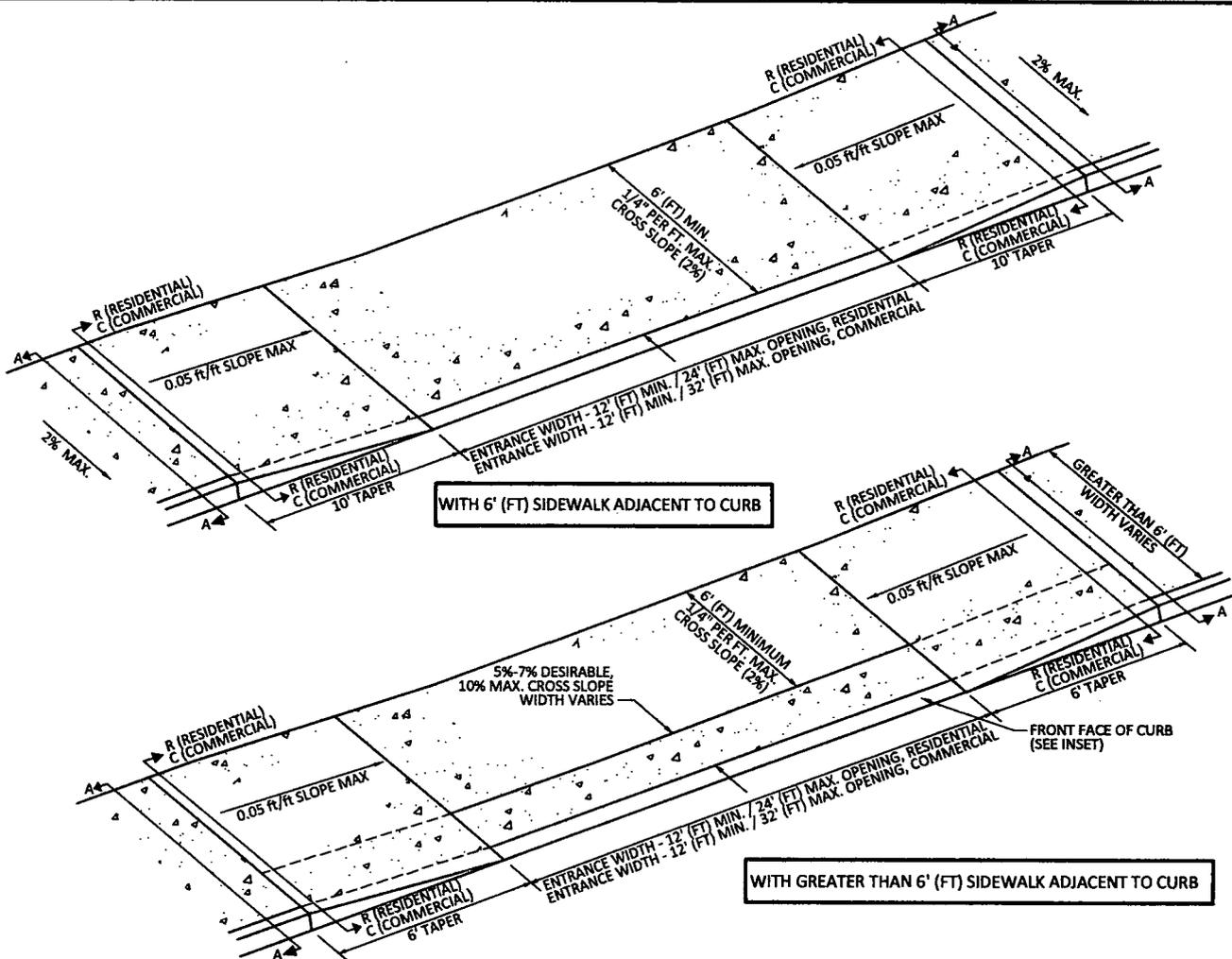
REVISED: REVISION ADOPTED BY CITY COUNCIL ON \_\_\_\_\_

REMARKS: FOR USE ON LOCAL, MSA, CSAH, OR FEDERAL PROJECTS

**SAINT PETER UTILITY STANDARD DETAIL**



V:\STPE\7017B.rpt - Proposed New Detail.dwg



NOTE:  
FOR CONSTRUCTION NOTES, REFER TO DETAILS 7020R (RESIDENTIAL) AND 7020C (COMMERCIAL).

**RESIDENTIAL / COMMERCIAL  
CONCRETE WALK AND APRON  
ADJACENT TO CURB**

NOT TO SCALE  
SAINT PETER STANDARD  
PLATE 7020A

**RESIDENTIAL / COMMERCIAL  
CONCRETE WALK AND APRON  
ADJACENT TO CURB**

STANDARD DETAIL  
PLATE NO:  
**7020A**

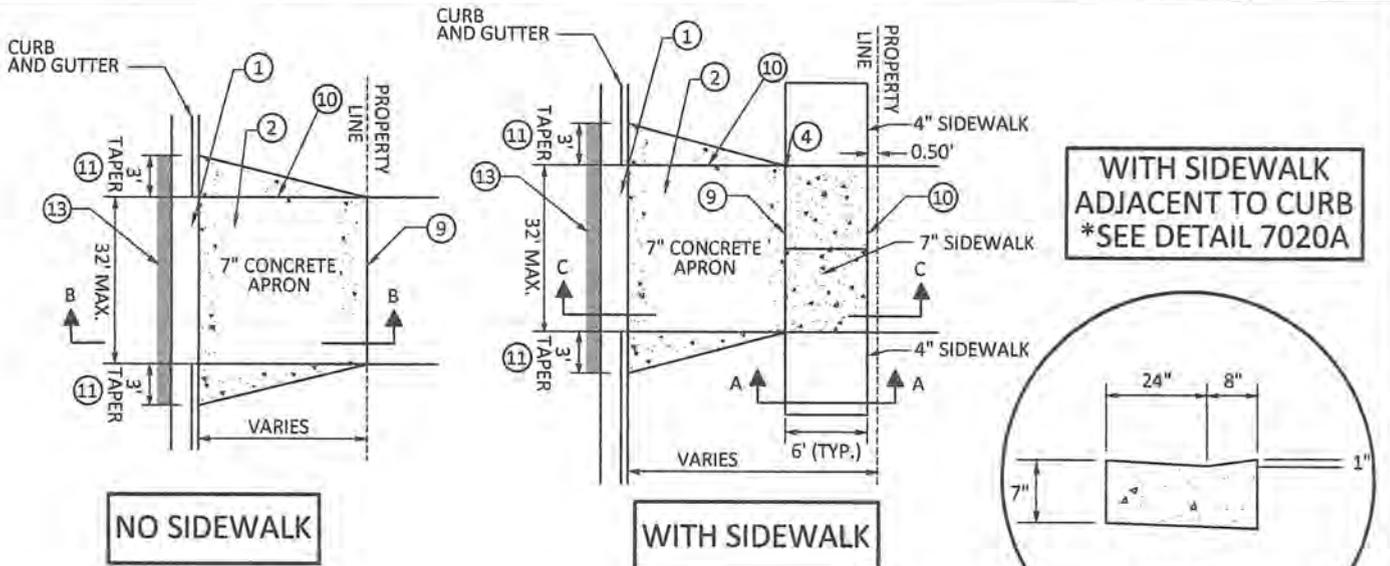
APPROVED: ADOPTED BY CITY COUNCIL ON APRIL 22, 2013

REVISED:

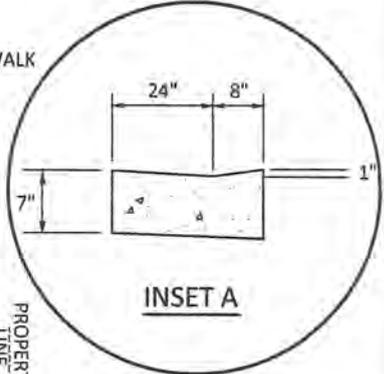
REMARKS:

**SAINT PETER UTILITY STANDARD DETAIL**

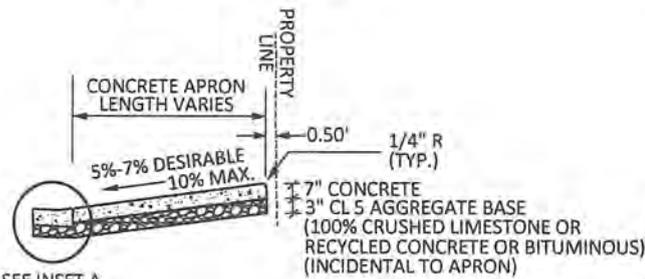




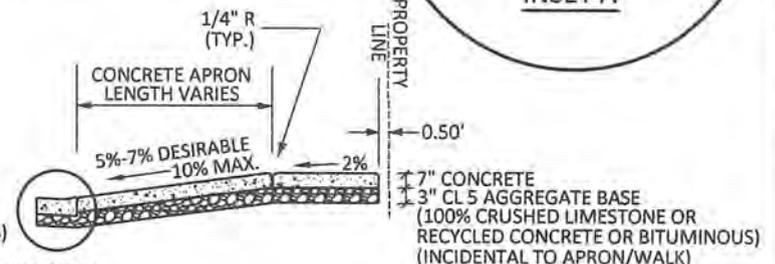
**WITH SIDEWALK  
ADJACENT TO CURB  
\*SEE DETAIL 7020A**



**INSET A**



SEE INSET A



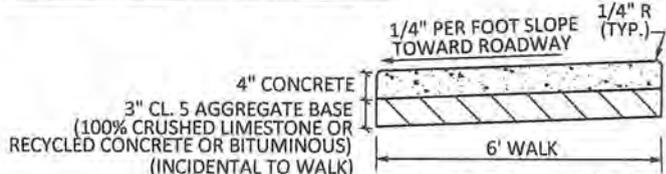
SEE INSET A

**SECTION B-B THROUGH APRON**

**SECTION C-C THROUGH APRON**

**NOTES:**

1. SEE "PEDESTRIAN CURB RAMP" DETAIL AND PLANS FOR PLACEMENT OF PEDESTRIAN CURB RAMP.
2. SLOPE OF DRIVEWAY TO PROPERTY LINE MAY VARY IF NO SIDEWALK IS REQUIRED.
3. MINNESOTA DEPARTMENT OF TRANSPORTATION STANDARD SPECIFICATIONS FOR CONSTRUCTION SHALL APPLY. CONCRETE MIX 3A32 WILL BE REQUIRED.
4. SIDEWALKS: REFERENCE SECTION 2521, CURB & GUTTER: REFERENCE SECTION 2531, DRIVEWAYS: REFERENCE SECTION 2531, AGGREGATE BASE: REFERENCE SECTION 2211
5. CONTRACTOR SEE PLANS FOR PLACEMENT OF WALK AND DIMENSIONS FOR CONSTRUCTION OF DRIVEWAYS.
6. 1/2" PRE FORMED JOINT FILLER MATERIAL-AASHTO M213 REQUIRED AT THE INTERSECTION OF ALL MAINLINE WALK WITH STATIONARY OBJECTS. (I.E., BUILDING FOUNDATION, EXISTING WALK, ETC.) (NOT REQUIRED WHEN CURB AND GUTTER OR WALK ARE ADJACENT TO BITUMINOUS PAVEMENT.)
7. CONCRETE DRIVEWAY PAVEMENT IS NOT TO BE POURED MONOLITHICALLY WITH CURB AND GUTTER.
8. DRIVEWAY OPENINGS SHALL BE A MAXIMUM OF 32' FROM BOTTOM OF 3' TAPER TO BOTTOM OF 3' TAPER.
9. 1/2" EXPANSION JOINT. PLACED AT 100 FT. INTERVALS FOR SIDEWALK.
10. CONTRACTION JOINT (FORMED OR SAWED)



**SECTION A-A THROUGH WALK**

11. IF A 6' (FT) WIDE WALK IS ADJACENT TO BACK OF CURB, THEN THE CURB TAPER SHALL BE 10' (FT) IN LENGTH. IF A WALK ADJACENT TO BACK OF CURB IS WIDER THAN 6' (FT) AND CONTAINS A MINIMUM OF 6' (FT) WALKING AREA AT 2% CROSS SLOPE, THE CURB TAPER SHALL BE 6' (FT) IN LENGTH. \* SEE DETAIL 7020A
12. CONCRETE WALK CONSTRUCTION JOINTS SHALL BE TOOLED OR SAW CUT AT 6 FT. INTERVALS.

13. SAW CUT AND REMOVE EXISTING BITUMINOUS PAVEMENT 6" MINIMUM FROM LIP OF GUTTER. PLACE BITUMINOUS PATCH TO MATCH EXISTING PAVEMENT SECTION.

**COMMERCIAL  
CONCRETE WALK AND APRON  
NOT TO SCALE  
SAINT PETER STANDARD  
PLATE 7020C**

**COMMERCIAL  
CONCRETE WALK AND APRON**

STANDARD DETAIL  
PLATE NO:  
**7020C**

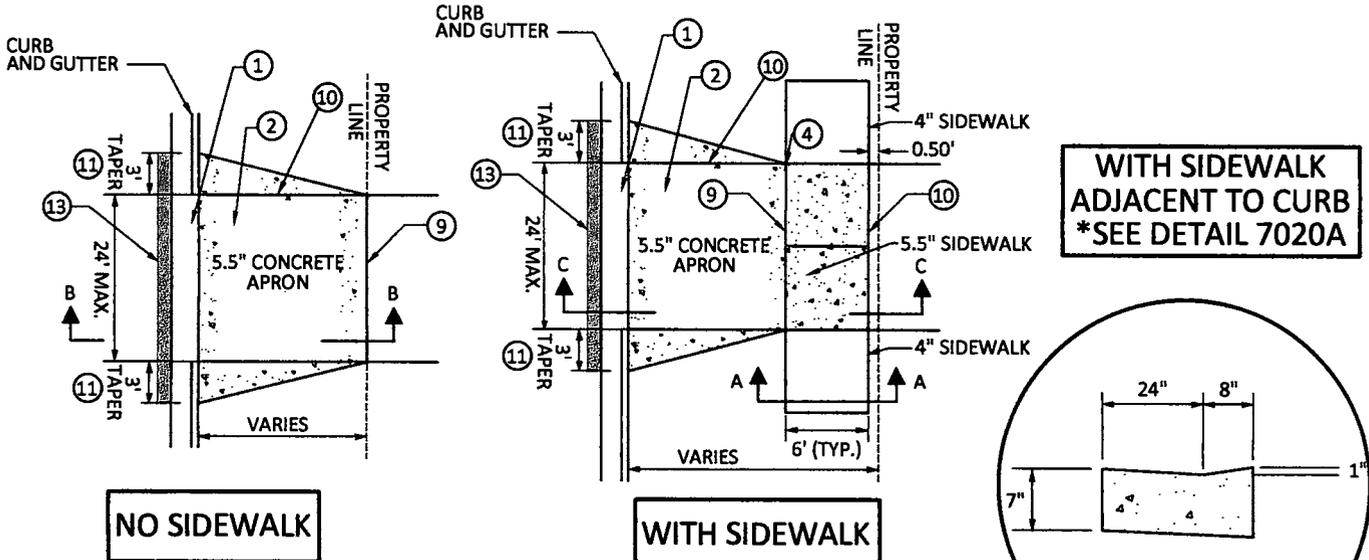
APPROVED: ADOPTED BY CITY COUNCIL ON JANUARY 12, 2004

REVISED: REVISION ADOPTED BY CITY COUNCIL ON APRIL 22, 2013

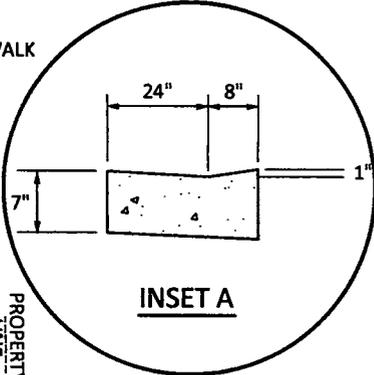
REMARKS:

**SAINT PETER UTILITY STANDARD DETAIL**





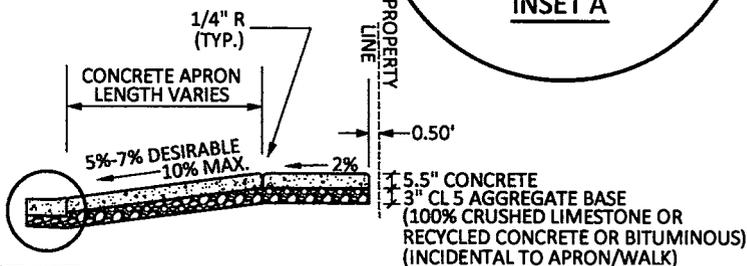
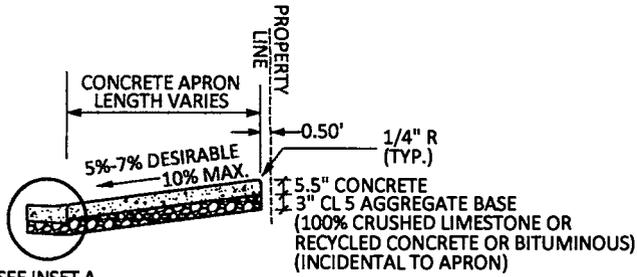
**WITH SIDEWALK  
ADJACENT TO CURB  
\*SEE DETAIL 7020A**



**NO SIDEWALK**

**WITH SIDEWALK**

**INSET A**

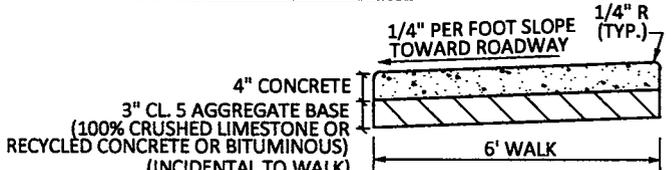


**SECTION B-B THROUGH APRON**

**SECTION C-C THROUGH APRON**

**NOTES:**

1. SEE "PEDESTRIAN CURB RAMP" DETAIL AND PLANS FOR PLACEMENT OF PEDESTRIAN CURB RAMP.
2. SLOPE OF DRIVEWAY TO PROPERTY LINE MAY VARY IF NO SIDEWALK IS REQUIRED.
3. MINNESOTA DEPARTMENT OF TRANSPORTATION STANDARD SPECIFICATIONS FOR CONSTRUCTION SHALL APPLY. CONCRETE MIX 3A32 WILL BE REQUIRED.
4. SIDEWALKS: REFERENCE SECTION 2521, CURB & GUTTER: REFERENCE SECTION 2531, DRIVEWAYS: REFERENCE SECTION 2531, AGGREGATE BASE: REFERENCE SECTION 2211
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7. CONCRETE DRIVEWAY PAVEMENT IS NOT TO BE POURED MONOLITHICALLY WITH CURB AND GUTTER.
8. DRIVEWAY OPENINGS SHALL BE A MAXIMUM OF 24' FROM BOTTOM OF 3' TAPER TO BOTTOM OF 3' TAPER.
9. 1/2" EXPANSION JOINT. PLACED AT 100 FT. INTERVALS FOR SIDEWALK.
10. CONTRACTION JOINT (FORMED OR SAWED)



**SECTION A-A THROUGH WALK**

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12. CONCRETE WALK CONSTRUCTION JOINTS SHALL BE TOOLED OR SAW CUT AT 6 FT. INTERVALS.

13. SAW CUT AND REMOVE EXISTING BITUMINOUS PAVEMENT 6" MINIMUM FROM LIP OF GUTTER. PLACE BITUMINOUS PATCH TO MATCH EXISTING PAVEMENT SECTION.

**RESIDENTIAL  
CONCRETE WALK AND APRON**  
NOT TO SCALE  
**SAINT PETER STANDARD  
PLATE 7020R**

**RESIDENTIAL  
CONCRETE WALK AND APRON**

STANDARD DETAIL  
PLATE NO:  
**7020R**

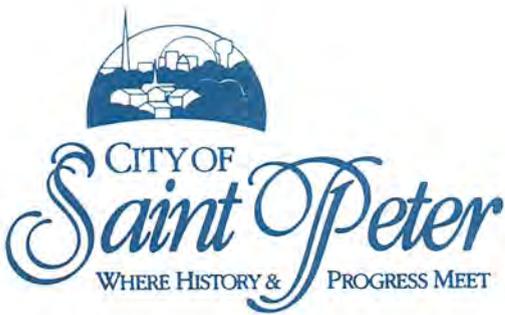
APPROVED: ADOPTED BY CITY COUNCIL ON JANUARY 12, 2004

REVISED: REVISION ADOPTED BY CITY COUNCIL ON APRIL 22, 2013

REMARKS:

**SAINT PETER UTILITY STANDARD DETAIL**





## Memorandum

**TO:** Honorable Mayor Strand  
Members of the City Council

**DATE:** 4/1/14

**FROM:** Todd Prafke  
City Administrator

**RE:** League of Minnesota Cities Annual Conference

### **ACTION/RECOMMENDATION**

None needed. For your discussion only.

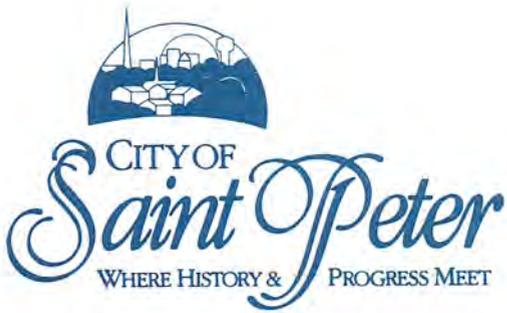
### **BACKGROUND**

The annual conference for the League of Minnesota Cities is being held in St. Cloud on June 18-20, 2014.

Time has been set aside on the workshop agenda for Monday evening to discuss which Councilmembers are interested in attending so that the City Administrator's office can register everyone for the conference and make hotel reservations.

Please feel free to contact me if you have any questions or concerns on this agenda item.

TP/bal



## Memorandum

**TO:** Honorable Mayor Strand  
Members of the City Council

**DATE:** 4/1/14

**FROM:** Todd Prafke  
City Administrator

**RE:** Goal Session Schedule

### **ACTION/RECOMMENDATION**

None needed. For your discussion only.

### **BACKGROUND**

Councilmembers are asked to bring their calendars to the workshop on Monday evening so that we can schedule the next Council goal session.

Please feel free to contact me if you have any questions or concerns on this agenda item.

TP/bal